

Speedtech **PERFORMANCE**

April 27, 2020

1967-1969 Camaro ExtReme Tunnel Cover

Part number 11510



Installation Instructions

The following instructions are intended for professional installers and are guidelines only. Speedtech Performance assumes NO responsibility for the installation of any of its products. All products must be installed by qualified professionals only. Read all instructions through before beginning.

Thank you for purchasing your new Speedtech Extreme Transmission Tunnel Cover. Before beginning the install, please read all instructions first to familiarize yourself with the process and tools required to complete the

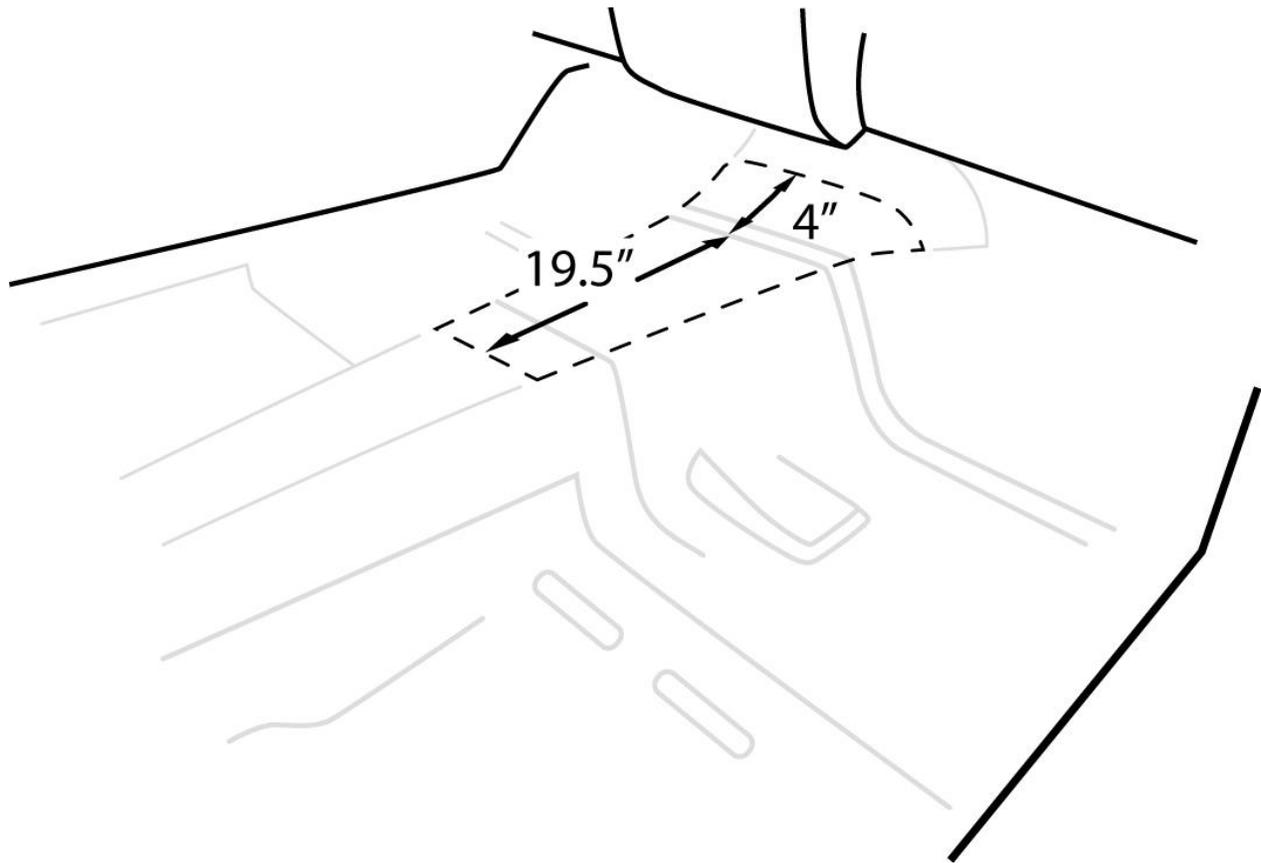
job. Take all necessary precautions whenever jacking up your vehicle and be sure to use safe and sturdy jack stands to support the vehicle whenever it is off the ground. Be sure to take necessary precautions when welding inside a vehicle and remove from the car any close-by flammable materials including the seats, carpet and insulation padding before performing this installation. Be sure to wear the proper protective gear when using power tools, and be sure to keep sparks away from glass and other interior components when grinding and welding.

This Tunnel Cover can be installed with basic hand and power tools and requires a slight bit of welding. You will also need two small pieces of scrap 16 ga. sheet metal (step 5) and seam sealer. You will be required to drill some holes, cut a hole in the floor for transmission clearance and a hole in the Tunnel Cover for the shifter. If you are unsure how to use the tools and materials and carryout the work required to install this cover, stop and seek a professional installer's help.

Fitment Note: This tunnel cover has been designed around the T56 Magnum aftermarket transmission and it's associated shifter locations. Some factory OEM transmissions put the shifter in different locations, for example Viper, '98-02 Camaro, GTO, etc. Some adjustment and/or modification to the tunnel and/or Tunnel Cover may be required in those cases. For your convenience a diagram of all tunnel measurements is at the end of these instructions.

1. Mark out your cut lines approximately where the vertical sides of the factory tunnel meet the horizontal top of the tunnel, and extend approximately 19.5" behind and 4" forward of the factory overlap pinch weld

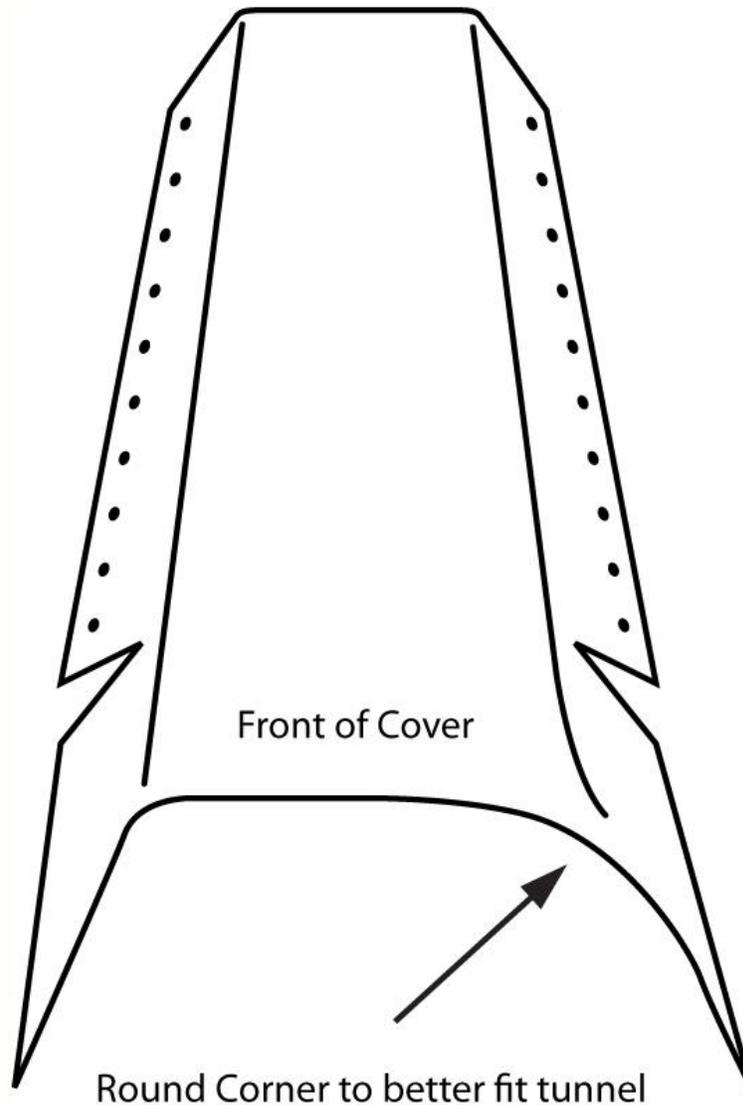
area, refer to the diagram below. Cut this portion of the tunnel out of the car. This should be close to the final fitment, although further minor trimming may be necessary. Be sure to leave enough metal for the tunnel cover to overlap the floor pan on all sides.



2. Find the final location for engine and transmission fitment within the car's frame. When installing the engine/ transmission we suggest you also install the headers so that you can adjust transmission tailshaft height position according to

header to floor clearance. We have found 1.5-2 degrees of driveshaft angle works best in Ext**R**eme chassis'. Adjust engine/ transmission angle with proper header clearance and drive shaft angle, this will give you your end shifter height.

3. Mock up the tunnel cover in place, the large open end goes towards the front. Some minor reshaping of the tunnel cover will be required to fit the contour of the floor. At the front of the tunnel cover reshape both corners of the folds to fit the contour of the bell housing area better, see below. *If you're installing our firewall kit at the same time this step isn't necessary, the firewall transition is made to fit to the unmodified shape of the tunnel cover.*



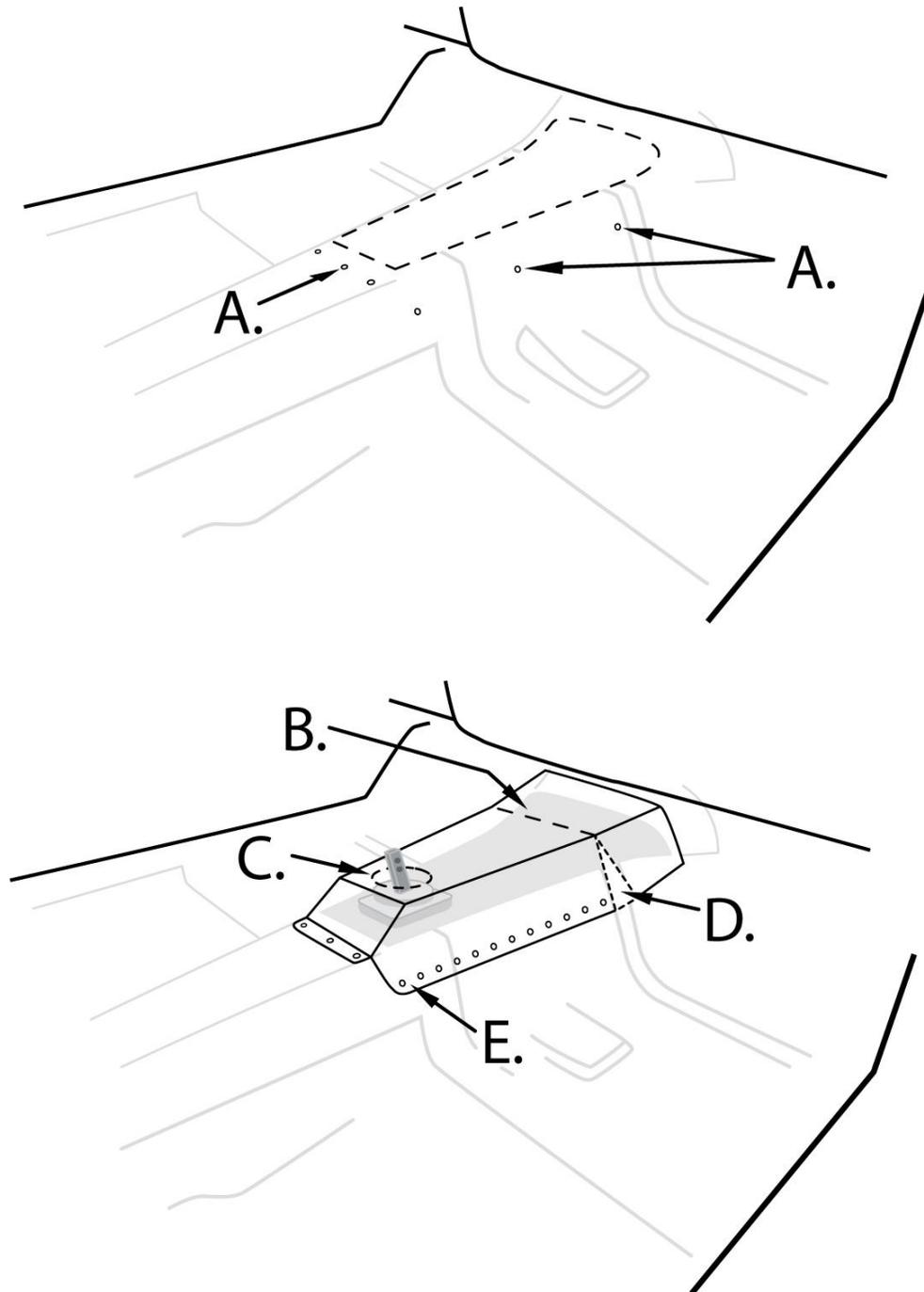
3. Once the tunnel cover is located properly and fits snug, mark and cut a hole approximately 3¾" (3.75) in diameter over where the shifter will be

located. See also (C) on the next page. Fit and attach the pre bent cap on the rearward end (left side as pictured) of the cover as seen below.



4. The front portion of the cover will need to be bent upward at the cut lines located on each side of the cover (D). This will help it better conform to the

upward rise of the car's floor pan and bell housing area, see (B) below. Some hammer and dolly reshaping of the floor and tunnel cover will now be necessary for a closer fit to each other. With everything fitting well together trace a few of the pre drilled holes (E) onto the floor pan (A) for future alignment purposes.



5. Bending the front portion of the cover upward will create a small pie shaped open area, see (D) on the previous page. These areas will need to

be filled with a scrap piece of 16 gauge steel sheet metal. We recommend welding it together outside of the car to prevent flying sparks and possible fires in your interior.

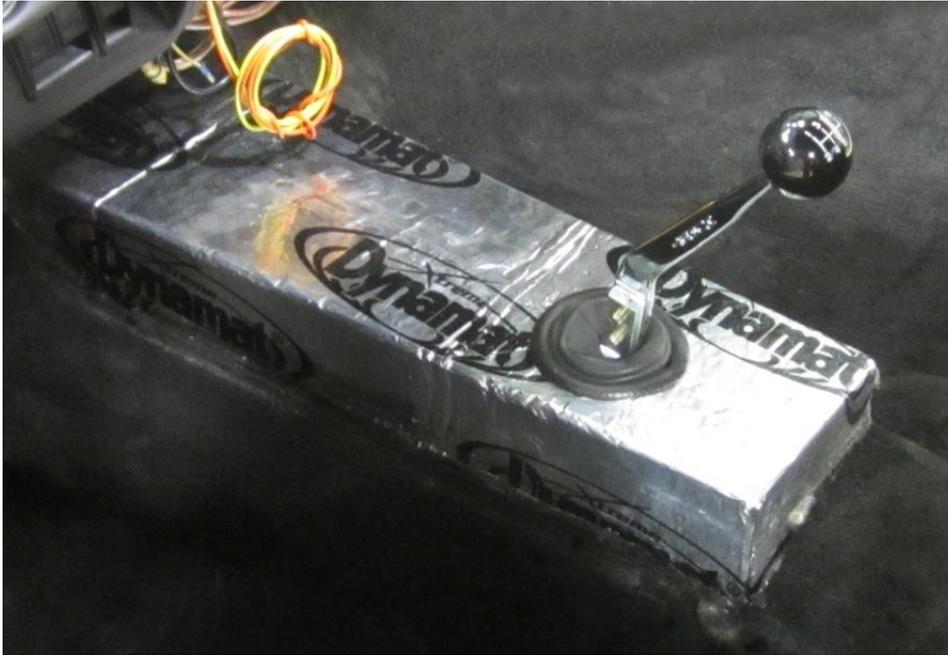
5. Once you feel comfortable with the fit of the tunnel cover, begin attaching the cover to the floor using the convenient pre-drilled holes, see **(E)** on the previous page. This may be done in several ways, for example plug welding, rivets, sheet metal screws, etc. There are no pre-drilled holes towards the front because there may be some trimming and shaping to fit different cars. You can drill these holes after modification is completed to help properly secure the front of the tunnel as well.

6. With the cover properly and securely fastened to the floor, be sure to seam seal all areas on top *and* underneath where the tunnel cover meets the floor. The end result will look similar to this cover installed in a 1970 Nova.



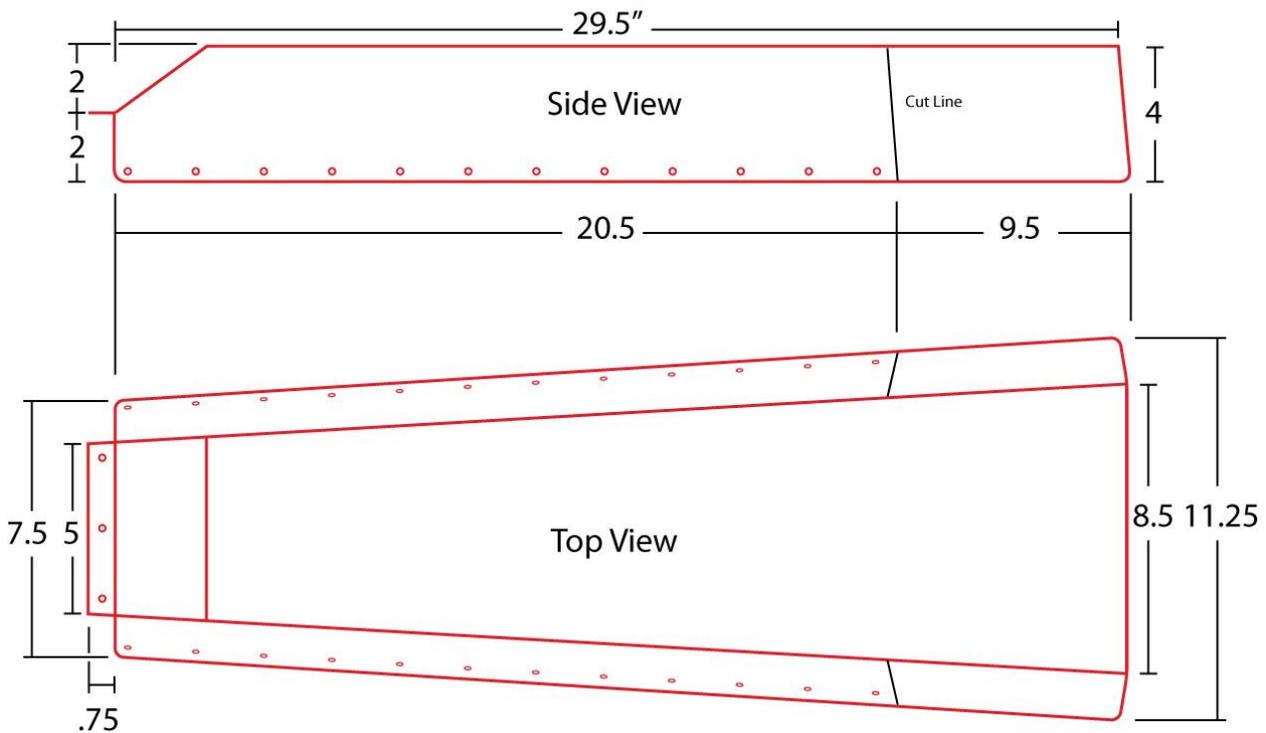
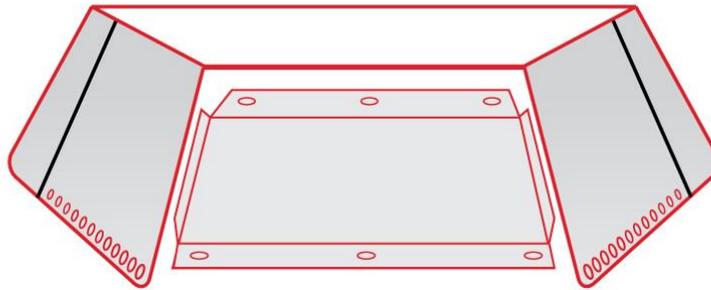
7. Cover your tunnel cover as fits your needs. We recommend splitting and securing vacuum tubing around the diameter of the shifter hole to prevent

the metal edge gouging the transmission's rubber shifter boot. Because the cover design is fairly low it will easily accommodate a custom center console.



11510
'67-69 Camaro w/T56
Extreme Tunnel Cover

Front Perspective



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